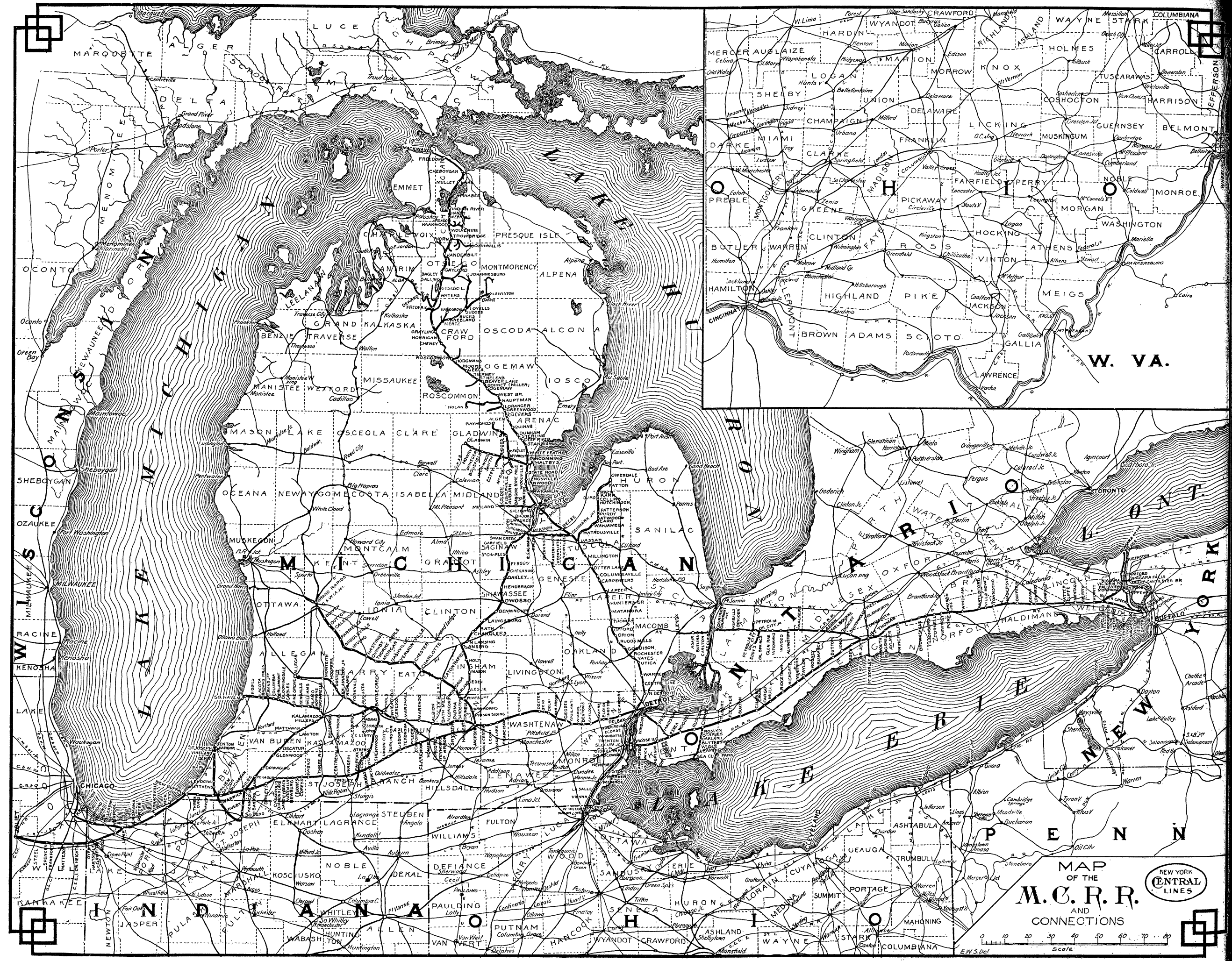


SIXTIETH
ANNUAL REPORT OF THE BOARD OF DIRECTORS
OF THE
MICHIGAN CENTRAL RAILROAD COMPANY
TO THE STOCKHOLDERS
FOR THE YEAR ENDED
DECEMBER 31 1905



DETROIT MICHIGAN

1906



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ANNUAL REPORT OF THE BOARD OF DIRECTORS
OF THE
MICHIGAN CENTRAL RAILROAD COMPANY
TO THE STOCKHOLDERS
FOR THE YEAR ENDED
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DETROIT MICHIGAN

1906

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INDEX

BALANCE SHEET	14, 15
CAPITAL STOCK	5, 15, 16
CAR MILEAGE	18
CARS:-FREIGHT, PASSENGER, BAGGAGE, ETC	21
COMMITTEE, EXECUTIVE	4
CONSTRUCTION ACCOUNT	6, 14
CORPORATE OFFICERS	4
COST OF ROAD AND EQUIPMENT	6
DIRECTORS	4
DIVIDENDS	13
EARNINGS IN DETAIL	7
ENGINE MILEAGE	18
EQUIPMENT	21
EXECUTIVE COMMITTEE	4
EXPENSES IN DETAIL	10, 11
FIRST CHARGES IN DETAIL	12, 13
FLOATING EQUIPMENT	21
FREIGHT STATISTICS	17, 18
FUNDED DEBT	5, 15, 16
INCOME ACCOUNT	7
INTEREST ON FUNDED DEBT	12
JACKSON, LANSING AND SAGINAW RAILROAD, LAND COMMISSIONER	22, 23
LAND COMMISSIONER, JACKSON, LANSING & SAGINAW RAILROAD	22, 23
LOCOMOTIVES	21
MILEAGE OF CARS	18
MILEAGE OF ENGINES	18
MILEAGE OF ROAD	5, 20
MILEAGE OF TRACKS	20
MILEAGE OF TRAINS	18
OFFICERS	4
OPERATIONS FOR THE YEAR	7, 8
OWNERSHIP IN OTHER LINES	14
PASSENGER STATISTICS	18, 19
PROFIT AND LOSS ACCOUNT	7
RENTALS OF LEASED LINES	12
ROAD OPERATED	5, 20
STOCKHOLDERS' ANNUAL MEETING	4
SUMMARY OF OPERATIONS	7
TAXES	7, 13
TRACKS, MILEAGE OF	20
TRAFFIC EARNINGS ANALYZED	17, 18, 19
TRAIN MILEAGE	18
TRANSFER OFFICE	16
TRANSPORTATION STATISTICS	17, 18, 19

ORGANIZATION OF THE MICHIGAN CENTRAL RAILROAD COMPANY

DIRECTORS

Elected at the annual meeting of stockholders held at Detroit, Michigan, May 4, 1905

HENRY B. LEDYARD, *Chairman*

WILLIAM K. VANDERBILT	WILLIAM H. NEWMAN
FREDERICK W. VANDERBILT	J. PIERPONT MORGAN
CHAUNCEY M. DEPEW	WILLIAM ROCKEFELLER
SAMUEL F. BARGER	JAMES STILLMAN
HAMILTON MCK. TWOMBLY	ASHLEY POND
WILLIAM K. VANDERBILT, JR.	GEORGE F. BAKER

EXECUTIVE COMMITTEE

HENRY B. LEDYARD, *Chairman of the Board of Directors* *

WILLIAM H. NEWMAN, *President* *

WILLIAM K. VANDERBILT	SAMUEL F. BARGER
FREDERICK W. VANDERBILT	HAMILTON MCK. TWOMBLY
CHAUNCEY M. DEPEW	

* *Members ex officio*

OFFICERS

<i>President</i>	WILLIAM H. NEWMAN
<i>Vice President</i>	EDWARD V. W. ROSSITER
<i>Vice President</i>	WILLIAM C. BROWN
<i>Vice President</i>	JOHN CARSTENSEN
<i>Vice President</i>	GEORGE J. GRAMMER
<i>Secretary</i>	DWIGHT W. PARDEE
<i>Treasurer</i>	CHARLES F. COX
<i>General Manager</i>	RICHARD H. L'HOMMEDIEU
<i>General Superintendent</i>	HENRY A. WORCESTER
<i>General Superintendent Motive Power, Rolling Stock and Machinery</i> }	JOHN F. DEEMS
<i>Chief Engineer</i>	GEORGE H. WEBB
<i>Auditor</i>	A. JUDSON BURT
<i>Purchasing Agent</i>	SYDNEY B. WIGHT
<i>Freight Traffic Manager</i>	BARRETT B. MITCHELL
<i>General Freight Agent</i>	WILLIAM C. ROWLEY
<i>Passenger Traffic Manager</i>	WARREN J. LYNCH
<i>General Passenger Agent</i>	OLIVER W. RUGGLES
<i>General Counsel</i>	ASHLEY POND
<i>General Attorney</i>	HENRY RUSSEL

The annual meeting of stockholders for the election of directors, is held in the city of Detroit, Michigan, on the Thursday following the first Wednesday in May.

REPORT

To the stockholders of

THE MICHIGAN CENTRAL RAILROAD COMPANY:

The Board of Directors herewith submits its report for the year ended December 31, 1905, with statements showing the results for the year and the financial condition of the company.

ROAD OPERATED

	<i>Miles</i>
Main line	270·07
Proprietary lines	343·91
Lines leased	1,117·34
Lines operated under trackage rights	14·00
Total road operated	<u>1,745·32</u>

A statement showing in detail the miles of road and track operated will be found upon page 20.

The capital stock authorized and issued to December 31, 1905, was	<u>\$18,738,000 00</u>
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The funded debt outstanding on December 31, 1904, was	\$24,325,000 00
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It has been increased during the year ended December 31, 1905,
as follows:

Michigan Central three and one half per cent gold bonds of 1902,	<u>1,000,000 00</u>
--	---------------------

\$25,325,000 00

Less—

Michigan Central—Jackson, Lansing & Saginaw three and one-half per cent gold bonds of 1901 purchased and cancelled by the Trustees of the Land Grant Fund of the Jackson, Lansing & Saginaw Railroad Company,

30,000 00

Total funded debt December 31, 1905 (see page 16)

\$25,295,000 00

On February 15, 1905, under an agreement entered into with the St. Joseph, South Bend & Southern Railroad Company and the Indiana, Illinois & Iowa Railroad Company, this company assumed the operation of a road between South Bend, Indiana, and St. Joseph, Michigan, a distance of 39.44 miles, at an annual rental of \$20,000. This agreement expires February 23, 1950, if not renewed upon notice one year before expiration. In connection with the operation of the above property under an agreement with the Indiana, Illinois & Iowa Railroad Company, the operation of a line between St. Joseph and Benton Harbor, a distance of 1.63 miles, was assumed, at an annual rental of \$5,000.

Under an agreement dated May 29, 1905, entered into between the Lansing Manufacturers' Railroad, the Lake Shore & Michigan Southern Railway Company and the Michigan Central Railroad Company, the switching road of the first named company, in Lansing, Michigan, was leased for a term of twenty-five years at a rental of \$7,500.00 per annum. This road is operated by the Michigan Central Railroad Company, the earnings and expenses being divided equally with the Lake Shore & Michigan Southern Railway Company.

By an agreement entered into January 1, 1905, the operation of that portion of the road of the Detroit, Toledo & Milwaukee Railroad Company from Homer to Allegan, a distance of 66.54 miles, was assumed. By the terms of the agreement, this company agrees to operate, maintain and pay all taxes and assessments during the corporate existence of the Milwaukee Company.

Cost of road and equipment on December 31st, 1904, was	\$34,640,708 19
--	-----------------

There was added during the year

New second track and diversions on main line between Kalamazoo and	
Niles in final completion of double track between Detroit and Chicago	723,129 89
Total cost of road and equipment to December 31st, 1905	<u>\$35,363,838 08</u>

This represents per mile owned (270.07 miles) \$130,943

There was expended on account of land purchases and engineering expenses in connection with the diversion of tracks at Barron Lake on the Air Line division and charged against the Michigan Air Line Railroad—cost of road and equipment	<u>\$20,794 39</u>
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SUMMARY OF FINANCIAL OPERATIONS AFFECTING INCOME

	1905	1904	Increase	Decrease
EARNINGS FROM OPERATION	1,745.32 miles operated	1,653.05 miles operated	92.27 miles	
From freight traffic	\$16,255,480 89	\$15,273,011 76	\$982,469 13	
From passenger traffic	5,225,528 26	4,818,763 54	406,764 72	
From express traffic	973,285 19	711,245 51	262,039 68	
From transportation of mails	453,557 56	452,955 10	602 46	
From rentals	72,562 50	*	72,562 50	
From miscellaneous sources	303,453 84	236,968 67	66,485 17	
Totals	\$23,283,868 24	\$21,492,944 58	\$1,790,923 66	
EXPENSES OF OPERATION	(81.03%)	(79.29%)	(1.74%)	
For maintenance of way and structures	\$3,418,740 27	\$3,135,124 50	\$283,615 77	
For maintenance of equipment	4,386,926 97	3,261,044 35	1,125,882 62	
For conducting transportation	10,518,814 40	10,130,381 83	388,432 57	
For general expenses	541,434 79	514,463 92	26,970 87	
TOTAL EXPENSES	\$18,865,916 43	\$17,041,014 60	\$1,824,901 83	
NET EARNINGS	\$4,417,951 81	\$4,451,929 98		\$33,978 17
OTHER INCOME	315,812 16	63,471 18	252,340 98	
GROSS INCOME	\$4,733,763 97	\$4,515,401 16	\$218,362 81	
FIRST CHARGES				
Interest on funded debt	\$2,015,320 29	\$1,989,663 33	\$25,656 96	
Rentals of leased lines	561,202 47	541,310 00	19,892 47	
Taxes	962,002 50	1,111,653 02		\$149,650 52
Interest on loans, notes and bills payable	221,784 99		221,784 99	
Totals	\$3,760,310 25	\$3,642,626 35	\$117,683 90	
BALANCE AVAILABLE FOR DIVIDEND	\$973,453 72	\$872,774 81	\$100,678 91	
Cash dividends, two, aggregating 4 per cent	749,520 00	749,520 00		
SURPLUS	\$223,933 72	\$123,254 81	\$100,678 91	
Surplus for the year				\$223,933 72
Amount to credit to Profit and Loss, December 31, 1904				8,640,223 22
		BALANCE, DECEMBER 31, 1905		\$8,864,156 94

*Included in earnings from miscellaneous sources in 1904

The gross earnings were \$23,283,868.24, an increase of \$1,790,923.66 over the previous year.

The freight earnings were \$16,255,480.89, an increase of \$982,469.13. This was due to an increased movement in nearly all commodities and in the average distance of each ton carried.

The passenger earnings were \$5,225,528.26, an increase of \$406,764.72, due to a general increase in both local and interline business.

The express earnings were \$973,285.19, an increase of \$262,039.68.

The expenses of operation were \$18,865,916.43, an increase of \$1,824,901.83.

Maintenance of way and structures showed an increase of \$283,615.77.

Maintenance of equipment increased \$1,125,882.62, due to renewal charges on account of cars and locomotives.

Conducting transportation increased \$388,432.57.

The principal fluctuations were as follows:

“Fuel for locomotives” shows an increase of \$224,234.91.

“Switching” charges show an increase of \$277,275.68. This increase is due to a change in methods of accounting.

Other miscellaneous accounts show an increase of \$322,036.20.

A total increase of \$823,546.79.

“Car mileage and per diem” shows a decrease of \$393,804.95.

“Rents for tracks, yards and terminals” shows a decrease of \$22,176.87; and miscellaneous accounts shows a decrease of \$19,132.40.

A total decrease of \$435,114.22.

Net increase, as above stated, in conducting transportation, \$388,432.57.

The net earnings were \$4,417,951.81, a decrease of \$33,978.17.

Other income was \$315,812.16, an increase of \$252,340.98, mainly due to increased revenue from other securities and interest on advances.

First charges, \$3,760,310.25, increased \$117,683.90, the principal increase being additional interest on Michigan Central First Mortgage Bonds issued, and interest on loans.

The profit from operation for the year, after payment of 4% in dividends upon the capital stock, was \$223,933.72, which has been carried to the credit of Income Account.

During the year the following changes in officers and directors have occurred:

January 31, 1905, the following officers were elected:

Chairman, Henry B. Ledyard, to succeed Chauncey M. Depew, resigned.

President, William H. Newman, to succeed Henry B. Ledyard, resigned.

Additional Vice-Presidents: William C. Brown, John Carstensen, George J. Grammer.

At the annual meeting of the stockholders, held May 4, 1905, the former Board of Directors was re-elected, with the exception of Frederick S. Winston, who was succeeded by George F. Baker. The vacancy caused by the death of Edwin D. Worcester was filled by William K. Vanderbilt, Jr., May 4, 1905.

February 1, 1905, Charles F. Daly was appointed Passenger Traffic Manager; he was succeeded December 1, 1905, by Warren J. Lynch.

November 15, 1905, Henry A. Worcester was appointed General Superintendent in the place of Sidney P. Hutchinson, resigned.

February 20, 1905, John F. Deems was appointed General Superintendent Motive Power, Rolling Stock and Machinery.

June 16, 1905, George H. Webb was appointed Chief Engineer to succeed Wilson S. Kinnear, promoted to position of Assistant General Manager.

W. H. NEWMAN, *President*.

EXPENSES IN DETAIL

MAINTENANCE OF WAY AND STRUCTURES

	1905	1904	Increase	Decrease
Superintendence	\$88,571 29	*	\$88,571 29	
Repairs of roadway	1,726,765 70	\$1,723,957 16	2,808 54	
Renewals of rails	444,562 00	319,713 36	124,848 64	
Renewals of ties	306,601 51	411,953 84		\$105,352 33
Repairs and renewals of bridges and culverts	312,762 44	196,769 66	115,992 78	
Repairs and renewals of fences, road crossings, { signs and cattle guards }	94,108 94	122,070 82		27,961 88
Repairs and renewals of buildings and fixtures	396,338 80	323,978 86	72,359 94	
Repairs and renewals of docks and wharves	27,637 53	14,595 13	13,042 40	
Repairs and renewals of telegraph	16,682 28	17,711 55		1,029 27
Stationery and printing	4,709 78	4,270 50	439 28	
Other expenses		103 62		103 62
Totals	\$3,418,740 27	\$3,135,124 50	\$283,615 77	

MAINTENANCE OF EQUIPMENT

Superintendence	\$75,134 37	\$71,509 11	\$3,625 26	
Repairs and renewals of locomotives	1,796,281 04	877,799 53	918,481 51	
Repairs and renewals of passenger cars	272,967 58	277,150 90		\$4,183 32
Repairs and renewals of freight cars	1,934,898 02	1,503,190 45	431,707 57	
Repairs and renewals of work cars	143,608 31	34,021 80	109,586 51	
Repairs and renewals of marine equipment	48,610 01	379,241 95		330,631 94
Repairs and renewals of shop machinery and tools	50,662 36	55,923 52		5,261 16
Stationery and printing	5,637 39	5,075 55	561 84	
Other expenses	59,127 89	57,131 54	1,996 35	
Totals	\$4,386,926 97	\$3,261,044 35	\$1,125,882 62	

CONDUCTING TRANSPORTATION

Superintendence	\$287,734 16	\$251,678 96	\$36,055 20	
Engine and roundhouse men	1,509,009 00	1,422,191 29	86,817 71	
Fuel for locomotives	1,956,012 32	1,731,777 41	224,234 91	
Water supply for locomotives	69,360 75	62,465 04	6,895 71	
Oil, tallow and waste for locomotives	65,143 97	55,902 72	9,241 25	
Other supplies for locomotives	12,325 02	13,247 91		\$922 89
Train service	960,341 76	912,092 86	48,248 90	
Train supplies and expenses	300,759 02	294,479 36	6,279 66	
Switchmen, flagmen and watchmen	805,200 42	796,726 92	8,473 50	
Telegraph expenses	257,650 21	257,973 34		323 13
Station service	1,244,486 03	1,238,942 33	5,543 70	
Station supplies	112,915 85	111,543 34	1,372 51	
Switching charges—balance †	453,964 15	176,688 47	277,275 68	
Car mileage and per diem—balance	770,595 97	1,164,400 92		393,804 95
Loss and damage	192,651 35	200,789 93		8,138 58
Injuries to persons	85,904 04	75,437 97	10,466 07	
Clearing wrecks	29,786 64	23,518 94	6,267 70	
Operating marine equipment	131,155 63	121,715 90	9,439 73	
Advertising	87,148 65	88,191 45		1,042 80
Carried forward	\$9,332,144 94	\$8,999,765 06	\$332,379 88	

*Included in other operating accounts in 1904

†Five months only in 1904

EXPENSES IN DETAIL (continued)

CONDUCTING TRANSPORTATION (continued)

	1905	1904	Increase	Decrease
<i>Brought forward</i>	\$9,332,144 94	\$8,999,765 06	\$332,379 88	
Outside agencies	307,640 44	284,732 00	22,908 44	
Stock yards and elevators	54,880 75	3,312 35	51,568 40	
Rents for tracks, yards and terminals	534,484 63	556,661 50		\$22,176 87
Rents of buildings and other property	26,230 89	34,935 89		8,705 00
Stationery and printing	124,685 86	116,262 36	8,423 50	
Other expenses	138,746 89	134,712 67	4,034 22	
Totals	<u>\$10,518,814 40</u>	<u>\$10,130,381 83</u>	<u>\$388,432 57</u>	

GENERAL EXPENSES

Salaries of general officers	\$144,317 52	\$130,982 62	\$13,334 90	
Salaries of clerks and attendants	148,322 11	133,101 82	15,220 29	
General office expenses and supplies	12,835 94	7,389 87	5,446 07	
Insurance	37,299 31	29,952 09	7,347 22	
Law expenses	58,514 60	42,693 79	15,820 81	
Stationery and printing (general offices)	12,891 33	11,578 31	1,313 02	
Other expenses	127,253 98	158,765 42		\$31,511 44
Totals	<u>\$541,434 79</u>	<u>\$514,463 92</u>	<u>\$26,970 87</u>	

PERCENTAGE OF OPERATING EXPENSES TO GROSS EARNINGS BY GROUPS

Maintenance of way and structures	14.68	14.59
Maintenance of equipment	18.84	15.17
Conducting transportation	45.18	47.13
General expenses	2.33	2.40
Totals	<u>81.03</u>	<u>79.29</u>

*FIRST CHARGES**Interest on bonds*

THIS COMPANY'S BONDS

Interest at 3½% per annum on Michigan Central First Mortgage Bonds	\$469,583 33	
Interest at 6 % per annum on Grand River Valley First Mortgage Bonds	30,000 00	
Interest at 6 % per annum on Grand River Valley First Mortgage Bonds	60,000 00	
Interest at 5 % per annum on Detroit & Bay City First Mortgage Bonds	200,000 00	
Interest at 5 % per annum on Kalamazoo & South Haven First Mortgage Bonds	35,000 00	
Interest at 4 % per annum on Michigan Air Line First Mortgage Bonds	104,000 00	
Interest at 4 % per annum on Terminal Railroad First Mortgage Bonds	29,000 00	
Interest at 3½% per annum on Jackson, Lansing & Saginaw First Mortgage Bonds	62,406 96	
Total		\$989,990 29

LEASED LINE BONDS

Interest at 3% per annum on Bay City & Battle Creek First Mortgage Bonds	\$7,500 00	
Interest at 3% per annum on Battle Creek & Sturgis First Mortgage Bonds	12,630 00	
Interest at 5% per annum on Canada Southern First Mortgage Bonds	700,000 00	
Interest at 5% per annum on Canada Southern Second Mortgage Bonds	300,000 00	
Interest at 4% per annum on Leamington and St. Clair Mortgage Bonds	5,200 00	
Total		\$1,025,330 00
Grand total		\$2,015,320 29

Rentals of leased lines

Jackson, Lansing & Saginaw rental	\$70,000 00	
Jackson, Lansing & Saginaw expenses	750 00	
Grand River Valley rental	24,560 00	
Joliet & Northern Indiana rental	71,000 00	
Canada Southern rental	375,000 00	
St. Joseph, South Bend and Southern rental	15,913 98	
Benton Harbor Extension, etc., rental	3,978 49	
Total		\$561,202 47

FIRST CHARGES—(*concluded*)*Taxes*

ON REAL ESTATE AND PROPERTY (INCLUDING LEASED LINES)

In the State of Michigan	\$820,257 25	
In the State of Indiana	53,910 06	
In the State of Illinois	46,053 82	
In the State of Ohio	7,660 71	
In the State of New York	6,781 52	
In the Dominion of Canada	24,155 93	
Excise tax, State of Ohio	3,183 21	
Total		<u>\$962,002 50</u>

Summary of first charges

Interest on funded debt	\$2,015,320 29
Rentals of leased lines	561,202 47
Taxes	962,002 50
Interest on loans, notes and bills payable	221,784 99
Total first charges	<u>\$3,760,310 25</u>

DIVIDENDS

Payable July 28, 1905, 2% on 187,380 shares of capital stock	\$374,760 00
Payable January 29, 1906, 2% on 187,380 shares of capital stock	374,760 00
Total <u>4%</u>	<u>\$749,520 00</u>

CONDENSED GENERAL BALANCE SHEET, DECEMBER 31, 1905

*Assets**Cost of road and equipment*

Michigan Central Railroad—Main Line		\$35,363,838 08
Michigan Air Line Railroad	\$3,299,652 23	
Grand River Valley Railroad	2,501,715 87	
Jackson, Lansing & Saginaw Railroad	2,629,921 64	
Kalamazoo & South Haven Railroad	815,610 24	
Detroit & Bay City Railroad	4,168,297 78	
Bay City & Battle Creek Railroad	7,171 75	
Battle Creek & Sturgis Railroad	330 59	
Terminal Railroad, Chicago	725,182 74	14,147,882 84
Total cost of road and equipment		<u>\$49,511,720 92</u>

Securities owned

Stock in sundry companies	\$6,831,540 49	
Bonds of sundry companies	353,975 00	7,185,515 49

Other property

Real estate, etc., not used in operation of the road		316,727 18
--	--	------------

Fuel and supplies

1,574,432 02

Current assets

Cash charged Treasurer and Local Treasurer	\$1,198,814 44	
Loans and bills receivable, Indiana Harbor R. R. Co.	2,321,347 28	
Loans and bills receivable, other companies	134,073 39	
Traffic balances receivable	2,883,163 22	
Sundry collectible accounts	1,038,095 24	7,575,493 57
		<u>\$66,163,889 18</u>

CONDENSED GENERAL BALANCE SHEET, DECEMBER 31, 1905

Liabilities

<i>Capital stock</i>		\$18,738,000 00
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Funded debt

Michigan Central 3½% bonds of 1952	\$14,000,000 00	
Michigan Air Line 4% bonds of 1940	2,600,000 00	
Grand River Valley 6% bonds of 1909	1,500,000 00	
Detroit & Bay City 5% bonds of 1931	4,000,000 00	
Kalamazoo & South Haven 5% bonds of 1939	700,000 00	
Terminal R. R. 4% bonds of 1941	725,000 00	
Jackson, Lansing & Saginaw 3½% bonds of 1951	1,770,000 00	25,295,000 00
Total capitalization		<u>\$44,033,000 00</u>

Current liabilities

Wages and supplies	\$96,175 78	
Loans and bills payable	4,450,000 00	
Traffic balances payable	280,934 96	
Interest and rentals accrued	348,865 24	
Interest unclaimed	29,265 00	
Dividend payable January 29, 1906	374,760 00	
Dividends unclaimed	8,171 00	
Sundry accounts payable	3,257,655 55	8,845,827 53

<i>Items in suspense</i>		4,420,904 71
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<i>Profit and loss</i>		8,864,156 94
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	<u>\$66,163,889 18</u>
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CAPITALIZATION

Capital stock

Number of shares issued and outstanding	{ 187,380	Total par value issued and out-standing	} \$18,738,000 00
Number of shares authorized	187,380	Total par value authorized	\$18,738,000 00
Par value per share	\$100 00		
Amount of capital stock per mile of road owned (270.07 miles) \$69,382.00			

Funded debt

Class of bond	Date of issue	Date of maturity	Amount of authorized issue	Amount issued and now outstanding	Rate of interest	Payable on the first days of
Michigan Central First Mortgage	{ 1902	May 1, 1952	\$18,000,000	\$14,000,000	3½%	{ May and November
Grand River Valley First Mortgage	{ 1879	September 1, 1909	500,000	500,000	6 %	{ March and September
Grand River Valley First Mortgage	{ 1886	September 1, 1909	1,000,000	1,000,000	6 %	{ March and September
Detroit & Bay City First Mortgage	{ 1881	March 1, 1931	4,000,000	4,000,000	5 %	{ March, June, September and December
Kalamazoo & South Haven First Mortgage	{ 1889	November 1, 1939	700,000	700,000	5 %	{ May and November
Michigan Air Line First Mortgage	{ 1890	January 1, 1940	2,600,000	2,600,000	4 %	{ January and July
Terminal Railroad First Mortgage	{ 1896	July 1, 1941	1,500,000	725,000	4 %	{ January and July
Jackson, Lansing & Saginaw First Mortgage	{ 1901	September 1, 1951	1,770,000	1,770,000	3½%	{ March and September
Total amount of funded debt				<u>\$25,295,000</u>		

Amount of funded debt per mile of road

Road	Funded debt	Miles	Amount per mile of road
Michigan Central Railroad	\$14,000,000	270.07	\$51,838
Grand River Valley Railroad	1,500,000	83.79	17,902
Detroit & Bay City Railroad	4,000,000	175.73	22,762
Kalamazoo & South Haven Railroad	700,000	39.50	17,722
Michigan Air Line Railroad	2,600,000	115.16	22,577
Terminal Railroad	725,000	15.34	47,262
Jackson, Lansing & Saginaw Railroad	1,770,000	379.23	4,667
	<u>\$25,295,000</u>	<u>1,078.82</u>	<u>\$23,447</u>

Treasurer, Grand Central Station, New York	{	Transfers stock
	{	Pays dividends on stock
	{	Transfers registered bonds
Guarantee Trust Company of New York	{	Pays interest on registered bonds
	{	Pays coupons from Michigan Central first mortgage bonds and bonds due 1951, secured by mortgage on the Jackson, Lansing & Saginaw Railroad
	{	Pays coupons from Joliet & Northern Indiana bonds
Farmers' Loan & Trust Company of New York		Pays coupons from all other issue of bonds
Union Trust Company of New York		

TRAFFIC STATISTICS

FREIGHT CARRIED AND TON MILEAGE

	1905	1904	Increase	Decrease
Tons of freight earning revenue	14,159,886	13,551,195	608,691	
Tons of company freight	2,115,593	1,871,945	243,648	
Total number of tons carried	16,275,479	15,423,140	852,339	
Tons carried one mile (revenue freight)	2,585,063,611	2,336,886,600	248,177,011	
Tons of company freight carried one mile	162,912,243	153,560,843	9,351,400	
Total number of tons carried one mile	2,747,975,854	2,490,447,443	257,528,411	

DESCRIPTION OF FREIGHT MOVED

PRODUCTS OF AGRICULTURE				
Grain	1,019,591	981,489	38,102	
Flour	191,261	174,934	16,327	
Other mill products	142,408	132,808	9,600	
Hay	81,789	176,815		95,026
Tobacco	13,676	11,082	2,594	
Cotton	23,161	37,977		14,816
Fruit and vegetables	205,308	259,574		54,266
Other articles	165,798	*	165,798	
PRODUCTS OF ANIMALS				
Live stock	374,027	333,582	40,445	
Dressed meats	214,546	176,814	37,732	
Other packing house products	124,839	170,616		45,777
Poultry, game and fish	19,532	20,105		573
Wool	7,483	9,552		2,069
Hides and leather	30,651	28,905	1,746	
Dairy products	48,972	*	48,972	
Other articles	4,243	*	4,243	
PRODUCTS OF MINES				
Anthracite coal	1,068,688	1,038,095	30,593	
Bituminous coal	2,693,393	2,741,113		47,720
Coke	156,034	107,466	48,568	
Ores	34,707	52,570		17,863
Stone, sand and other like articles	1,163,234	919,656	243,578	
Other articles	197,424	*	197,424	
PRODUCTS OF FOREST				
Lumber	921,430	1,175,487		254,057
Other articles	1,111,512	*	1,111,512	
MANUFACTURES				
Petroleum and other oils	113,186	110,579	2,607	
Sugar	93,937	113,415		19,478
Iron: pig and bloom	143,032	137,622	5,410	
Iron and steel rails	15,534	26,860		11,326
Other castings and machinery	682,429	544,860	137,569	
Bar and sheet metal	264,888	103,273	161,615	
Cement, brick and lime	367,933	382,595		14,662
Agricultural implements	70,798	51,966	18,832	
Wagons, carriages, tools, etc	95,951	62,758	33,193	
Wines, liquors and beers	37,088	36,204	884	
Household goods and furniture	44,051	39,476	4,575	
Other articles	1,029,627	*	1,029,627	
MERCHANDISE				
MISCELLANEOUS				
Other commodities not previously mentioned	1,187,725	3,392,947		2,205,222
TOTAL	14,159,886	13,551,195	608,691	

*Included in other commodities in 1904

TRAFFIC STATISTICS (continued)

PASSENGERS CARRIED AND TICKET MILEAGE

	1905	1904	Increase	Decrease
Interline passengers	684,869	688,876		4,007
Local passengers	3,083,326	2,812,204	271,122	
Commutation passengers	155,671	155,930		259
Total number of passengers carried	3,923,866	3,657,010	266,856	
Passengers carried one mile	249,329,847	229,794,138	19,535,709	

TRAIN MILEAGE

Freight trains	6,035,214	5,764,698	270,516	
Passenger trains, exclusive of straight mail and express trains	5,423,398	4,693,419	729,979	
Mail and express trains exclusive	274,871	263,559	11,312	
Mixed trains	155,088	527,036		371,948
Work trains	252,435	248,880	3,555	
Total train mileage	12,141,006	11,497,592	643,414	

ENGINE MILEAGE

Freight engines	7,846,689	7,593,376	253,313	
Passenger engines	5,971,627	5,696,571	275,056	
Switching engines	4,552,963	4,322,044	230,919	
Work engines	692,246	579,927	112,319	
Total engine mileage	19,063,525	18,191,918	871,607	

CAR MILEAGE

Loaded freight cars	165,381,853	157,232,784	8,149,069	
Empty freight cars (including caboose cars)	69,720,167	65,531,831	4,188,336	
Total freight cars	235,102,020	222,764,615	12,337,405	
Passenger cars	34,051,649	31,585,999	2,465,650	
Work cars	3,595,377	3,870,254		274,877
Total car mileage	272,749,046	258,220,868	14,528,178	
Miles of road, including trackage, operated in freight service	1,745.32	1,653.05	92.27	
Miles of road, including trackage, operated in passenger service	1,699.63	1,592.02	107.61	

FREIGHT RESULTS

Earnings from transportation	\$15,899,834 59	\$14,969,635 14	\$930,199 45	
from miscellaneous sources	355,646 30	303,376 62	52,269 68	
Total freight earnings	\$16,255,480 89	\$15,273,011 76	\$982,469 13	
Earnings per ton	\$1.12	\$1.10	\$0.02	
Earnings per ton per mile	cts. 0.615	cts. 0.641		cts. 0.026
Earnings per train mile	\$2.57	\$2.38	\$0.19	
Earnings per mile of road, trackage included, operated in freight service	\$9,313.75	\$9,239.29	\$74.46	
Density of freight traffic (i. e. tons carried one mile per mile of road	1,574,483	1,506,577	67,906	
Average number of tons of revenue earning freight carried per train mile	418	371	47	
Average number of tons of all freight (including company's) carried per train mile	444	396	48	
Average number of revenue tons per loaded car mile	16	15	1	
Average number of all tons per loaded car mile	17	16	1	
Average number of miles one ton carried	169	161	8	
Average number of loaded cars per train mile	26.72	24.99	1.73	
Average number of empty cars per train mile	11.26	10.41	.85	
Average number of freight cars per train mile	37.98	35.40	2.58	

TRAFFIC STATISTICS (concluded)

PASSENGER RESULTS

	1905	1904	Increase	Decrease
Earnings from passengers	\$5,137,362 27	\$4,729,124 42	\$408,237 85	
Other passenger train earnings	80,478 19	82,230 17		1,751 98
From miscellaneous sources	7,687 80	7,408 95	278 85	
Total passenger earnings	\$5,225,528 26	\$4,818,763 54	\$406,764 72	
Earnings from mail and express	\$1,426,842 75	\$1,164,200 61	\$262,642 14	
Earnings per passenger	\$1.31	\$1.29	\$0.02	
Earnings per passenger per mile	cts. 02.06	cts. 02.06		
Earnings per train mile	\$0.94	\$0.92	\$0.02	
Earnings per train mile, including mail and express earnings	\$1.14	\$1.09	\$0.05	
Earnings per mile of road operated in passenger service, trackage included (including mail and express)	\$3,914.01	\$3,758.10	\$155.91	
Density of passenger traffic (<i>i. e.</i> passengers carried one mile per mile of road)	146,697	144,341	2,356	
Average number of passenger cars per train mile	5.82	5.76	.06	
Average number of passengers per train mile	45	44	1	
Average number of miles one passenger carried	64	63	1	

TABLE OF TRACKS

MAIN LINE	Miles	Second track	Third track	Fourth track	Sidings	Total
Kensington to Detroit	270.07	270.07	3.00	3.00	345.40	891.54
PROPRIETARY LINES						
Detroit & Bay City Railroad	109.00	5.54			41.55	156.09
Bay City & Battle Creek Railway	18.00				7.50	25.50
Battle Creek & Sturgis Railway	33.80				3.73	37.53
Detroit, Delray & Dearborn Railroad	4.84				1.41	6.25
Joliet & Northern Indiana Railroad	45.00				25.69	70.69
Detroit, Toledo & Milwaukee Railroad	66.54				10.00	76.54
<i>Total proprietary lines</i>	277.18	5.54			89.88	372.60
BRANCHES						
On Detroit & Bay City Railroad	66.73				51.94	118.67
<i>Total proprietary lines and branches</i>	343.91	5.54			141.82	491.27
LEASED LINES						
Michigan Air Line Railroad	115.16				43.13	158.29
Grand River Valley Railroad	83.79				17.41	101.20
Kalamazoo & South Haven Railroad	39.50				7.01	46.51
Jackson, Lansing & Saginaw Railroad	295.10	3.44			268.53	567.07
Detroit Manufacturers' Railroad	1.29				1.24	2.53
St. Joseph, South Bend & Southern Railroad	39.44				7.04	46.48
Benton Harbor Extension	1.63					1.63
Canada Southern Railway (in Canada)	226.18	150.47			139.73	516.38
Canada Southern Bridge Company	3.66				1.55	5.21
Michigan Midland & Canada Railway	14.68				1.40	16.08
Toledo, Canada Southern & Detroit Railway	55.87	3.40			91.04	150.31
<i>Total leased lines</i>	876.30	157.31			578.08	1,611.69
BRANCHES						
On Jackson, Lansing & Saginaw Railroad	84.13				66.35	150.48
On Toledo, Canada Southern & Detroit Railway	3.05				8.89	11.94
On Canada Southern Railway (in Canada)	153.86				43.36	197.22
	241.04				118.60	359.64
<i>Total leased lines and branches</i>	1,117.34	157.31			696.68	1,971.33
<i>Total main line proprietary and leased lines</i>	1,731.32	432.92	3.00	3.00	1,183.90	3,354.14
LINES OPERATED UNDER TRACKAGE RIGHTS						
Illinois Central Railroad, Kensington to Chicago	14.00					14.00
<i>Grand total, all lines operated</i>	1,745.32	432.92	3.00	3.00	1,183.90	3,368.14
<i>Recapitulation according to States</i>						
Michigan	Main line 221.00	Proprietary 298.91		Leased 705.42		Total 1,225.33
Illinois	6.07	29.00			14.00	49.07
Indiana	43.00	16.00		20.23		79.23
Ohio				11.65		11.65
Canada				380.04		380.04
	270.07	343.91		1,117.34	14.00	1,745.32

Gauge of track, 4 feet 8½ inches. Weight of rail, per yard, 52 to 100 pounds

EQUIPMENT

OWNED

LOCOMOTIVES

	1905	1904
For passenger service	126	132
For freight service	209	204
For switching service	126	125
Total locomotives in service	461	461
Average mileage per engine	41,325	42,857
Cost of repairs (excluding renewals) per engine mile	cts. 4.31	cts. 4.46
Cost of fuel per ton	\$1.985	\$1.950
Cost of fuel per engine mile	cts. 9.33	cts. 8.82

CARS IN PASSENGER SERVICE

Passenger coaches	112	112
Smoking cars	18	18
Combination cars	30	28
Immigrant and excursion cars	78	77
Dining cars	14	14
Buffet and cafe cars	10	10
Mail, express and baggage cars	111	110
Officers' and pay cars	6	6
Other cars in passenger service	9	8
Totals	388	383
Total seating capacity of revenue passenger cars	14,477	14,300
Average mileage per passenger car	67,279 miles	62,182 miles
Cost of repairs (excluding renewals) per passenger car mile	cts. 00.818	cts. 00.977

CARS IN FREIGHT SERVICE

Box cars	9,351	9,509
Flat cars, common	2,324	2,055
Flat cars, steel	95	97
Stock cars	585	593
Coal and coke cars	2,194	2,016
Refrigerator and produce cars	434	102
Caboose cars	285	278
Totals	15,268	14,650
Total capacity of freight cars	585,435 tons	417,545 tons
Average capacity of freight cars	38.34 tons	28.50 tons
Average mileage of freight cars	15,398 miles	15,202 miles
Cost of repairs (excluding renewals) per freight car mile	cts. 00.343	cts. 00.316

CARS IN COMPANY'S SERVICE

Ballast cars	187	113
Derrick cars	4	4
Steam wrecking cranes	7	5
Cinder, push, gas and oil transport cars	46	35
Other road cars	349	290
Totals	593	447

MARINE DEPARTMENT

Ferryboats		
Side wheel, single deck	3	3
Screw wheel, single deck	1	1
Totals	4	4

*REPORT OF THE LAND COMMISSIONER OF THE JACKSON, LANSING &
SAGINAW RAILROAD COMPANY*

MR. W. H. NEWMAN

DETROIT, MICH., JANUARY 31st, 1906

President Michigan Central Railroad

NEW YORK

DEAR SIR—I respectfully submit herewith annual statement of the business of the Land Department of this company for the year ending December 31st, 1905

LANDS AND SALES

	LANDS	Acres	Amount
Unsold January 1st, 1905, according to patents	110,280.88		
Restored to market in 1905,	118.05		
	<hr/>	110,398.93	
Sold during the year		3,904.15	
		<hr/>	
Unsold at the close of the year		106,494.78	
		<hr/>	
Sales			
Lands			\$14,646 46
Timber			50 00
			<hr/>
			\$14,696 46

LAND CONTRACTS

Total amount due on contracts at the close of the year	\$26,813 70
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The sales for the last five years are as follows

	1901	1902	1903	1904	1905
Acres sold	6,883.87	9,217.56	23,712.10	8,076.33	3,786.10
Average per acre	\$8 63	\$7 64	\$0 93	\$2 62	\$3 87
	<hr/>	<hr/>	<hr/>	<hr/>	<hr/>
Land sales	\$59,453 27	\$70,384 11	\$22,014 40	\$21,137 96	\$14,646 46
Timber sales					50 00
	<hr/>	<hr/>	<hr/>	<hr/>	<hr/>
Total	\$59,453 27	\$70,384 11	\$22,014 40	\$21,137 96	\$14,696 46
	<hr/>	<hr/>	<hr/>	<hr/>	<hr/>

*REPORT OF THE LAND COMMISSIONER OF THE JACKSON, LANSING &
SAGINAW RAILROAD COMPANY (concluded)*

RECEIPTS

Cash on hand January 1st, 1905	\$3,435 33	
From payments on land contracts and sales	13,861 45	
From interest	837 42	
Total		<u>\$18,134 20</u>

DISBURSEMENTS

Deposited to the credit of trustees	\$6,056 39	
For taxes	2,005 60	
For salaries and commissions	3,503 29	
For explorations	132 85	
For legal expenses	384 81	
For miscellaneous expenses	231 32	
Balance	5,819 94	
Total		<u>\$18,134 20</u>

NATHANIEL E. SLAYMAKER

Land Commissioner

Messrs. Ledyard, Pond and Joy, Trustees

Balance on hand end of 1904, as shown by report for that year	\$25,284 10	
Amount received from land commissioner during 1905	6,056 39	
Interest on land fund year 1905	262 08	
Total	<u>\$31,602 57</u>	
Bonds purchased and cancelled during 1905	29,593 75	
Cash on hand December 31, 1905		<u>\$2,008 82</u>

